

REGULAR MEETING OF THE LAND USE AND PLANNING COMMITTEE

Notice is hereby given of the following Land Use and Planning Committee:
June 12, 2017 at 12:00 p.m.
Committee Room 2, City Hall

AGENDA

ADDITIONS / DELETIONS TO THE AGENDA

MOTION to amend the Land Use and Planning Committee agenda.

ADOPTION OF MINUTES

- 1. Adoption of the following minutes:**
 - a. April 10, 2017
 - b. May 1, 2017
 - c. May 15, 2017

PRESENTATIONS

- 2. No Items**

UNFINISHED BUSINESS

- 3. No Items**

REPORTS FOR ACTION

- 4. 800 Columbia Street: Rezoning and Special Development Permit to Allow Liquor Primary Licensed Premise – Preliminary Report**
- 5. 1050 Boyd Street: Development Permit Application – Preliminary Report**

DIRECTOR'S / MANAGER'S REPORT (Oral Report)

6.

NEW BUSINESS

7.

CORRESPONDENCE

8. No Items

ADJOURNMENT



REGULAR MEETING OF THE LAND USE AND PLANNING COMMITTEE

April 10, 2017 at 11:00 a.m.
Committee Room 2, City Hall

MINUTES

PRESENT:

Mayor Jonathan Coté
Councillor Patrick Johnstone
Councillor Jaimie McEvoy

STAFF:

Ms. Jackie Teed	- Acting Director of Development Services
Mr Jim Hurst	- Planner
Mr Mike Watson	- Planner
Mr John Stark	- Acting Manager of Planning
Ms. Heather Corbett	- Committee Clerk

The meeting was called to order at 11:17 a.m.

ADDITIONS / DELETIONS TO THE AGENDA

There were no additions.

ADOPTION OF MINUTES

1. a. Adoption of the December 5, 2016 Minutes:

MOVED and SECONDED

THAT the December 5, 2016 Land Use and Planning Committee minutes be adopted.

CARRIED.

All members of the Committee present voted in favour of the motion.

b. Adoption of the March 6, 2017 Minutes:

MOVED and SECONDED

THAT the March 6, 2017 Land Use and Planning Committee minutes be adopted.

CARRIED.

All members of the Committee present voted in favour of the motion.

PRESENTATIONS

2. No Items

UNFINISHED BUSINESS

3. No Items

REPORTS FOR ACTION

4. 30 Royal Avenue and 55 Dufferin Street: Pre- Application Review

Mike Watson, Planner, summarized the report dated April 10 regarding a pre-application review for 30 Royal Avenue and 55 Dufferin Street.

In response to questions from the Committee, Mr. Watson provided the following information:

- The land use designation in the OCP for the area of this site is Residential – Mid-Rise Apartment.

Discussion ensued and the Committee noted the following comments:

- If the application is pursued, it would be prudent to reach out to Translink in regards to future Patullo Bridge on-ramp locations; and
- The Committee had expected a six storey wood frame building in this designation and would be hesitant to veer away from that form and set precedent in this regard.

The LUPC directed staff to include feedback to the applicant that an application consistent with either approach described in section 5.4 of the April 10, 2017 report would be considered.

MOVED and SECONDED

THAT the staff report dated April 10, 2017 be received for information.

CARRIED.

All members of the Committee present voted in favour of the motion.

5. 800 Block Agnes Street (810 and 824 Agnes Street and 815 and 821 Victoria Street): Pre-Application Review

Mike Watson, Planner, summarized the report dated April 10 regarding a pre-application review for 800 Block Agnes Street (810 and 824 Agnes Street and 815 and 821 Victoria Street).

Discussion ensued, and the Committee noted the following comments:

- There were concerns expressed regarding the proposed sale of the City property at 824 Agnes Street formerly owned by the Chinese Benevolent Association (CBA), currently slated for park space;
- There were no concerns expressed regarding density at the site, especially when so close to Skytrain; and
- Consideration of a non-profit arrangement for the proposed child care space would be well received.

The LUPC directed staff to include feedback to the applicant that an application consistent with approach Two (Consider Extra Density beyond the Density Bonus Phase 2 Policy) for 800 Block Agnes Street, as outlined in section 5.4 of the April 10, 2017 report would be considered.

MOVED and SECONDED

THAT the staff report dated April 10, 2017 be received for information.

CARRIED.

All members of the Committee present voted in favour of the motion.

- 6. 1102, 1110, 1116 and 1122 Salter Street: Official Community Plan Amendment and Rezoning to Allow a 78 Unit Residential Development, Park and Road Dedication - Bylaws for First and Second Readings**

MOVED and SECONDED

THAT the Land Use and Planning Committee recommend that Council consider Official Community Plan Amendment Bylaw 7916, 2017 and Zoning Amendment Bylaw 7917, 2017 for First and Second Reading and forward the Bylaws to a Public Hearing on May 29, 2017.

CARRIED.

All members of the Committee present voted in favour of the motion.

- 7. 302 Twelfth Street (Key West Ford): Development Permit Application - Preliminary Report**

MOVED and SECONDED

THAT the Land Use and Planning Committee recommend that staff process the Development Permit application as outlined in the report dated April 10, 2017.

CARRIED.

All members of the Committee present voted in favour of the motion.

DIRECTOR'S / MANAGER'S REPORT (Oral Report)
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- 8. No Items**

NEW BUSINESS

9. No Items

CORRESPONDENCE

10. No Items

ADJOURNMENT

ON MOTION, the meeting was adjourned at 11:41 a.m.

JONATHAN COTÉ
MAYOR

HEATHER CORBETT
COMMITTEE CLERK



Corporation of the City of
NEW WESTMINSTER

REGULAR MEETING OF THE LAND USE AND PLANNING COMMITTEE

May 1, 2017 at 12:00 p.m.
Committee Room 2, City Hall

MINUTES

PRESENT:

Mayor Jonathan Coté
Councillor Patrick Johnstone
Councillor Chuck Puchmayr

STAFF:

Mr. Rupinder Basi	- Senior Planner
Mr. David Guiney	- Senior Planning Analyst
Ms. Jackie Teed	- Acting Director of Development Services
Ms. Carrie Peacock	- Recording Secretary, Raincoast Ventures Ltd.

The meeting was called to order at 12:00 p.m.

ADDITIONS / DELETIONS TO THE AGENDA

No items presented.

ADOPTION OF THE MINUTES

1. No items.

PRESENTATIONS

2. No items.

UNFINISHED BUSINESS

3. No items.

REPORTS FOR ACTION

4. **628 Eighteenth Street: DVP00627 to Vary Accessory Site Coverage in Order to Construct a Pool and a Hot Tub – Preliminary Report**

MOVED and SECONDED

THAT the Land Use and Planning Committee recommends Council issue notice that it will consider a resolution to issue Development Variance Permit DVP00627 to permit accessory site coverage of 19 percent at 628 Eighteenth Street so as to enable the property owners to construct an in-ground swimming pool and a hot tub, following an Opportunity to Be Heard on June 26, 2017.

CARRIED.

All members of Council present voted in favour of the motion.

DIRECTOR'S / MANAGER'S REPORT

5. No items.

NEW BUSINESS

6. No items.

ADJOURNMENT

ON MOTION, the meeting was adjourned at 12:02 p.m.

JONATHAN COTÉ
MAYOR

CARRIE PEACOCK
RECORDING SECRETARY



REGULAR MEETING OF THE LAND USE AND PLANNING COMMITTEE

May 15, 2017 at 12:00 p.m.
Committee Room 2, City Hall

MINUTES

PRESENT:

Mayor Jonathan Coté
Councillor Patrick Johnstone
Councillor Jaimie McEvoy

STAFF:

Mr. Jim Hurst	- Planner
Mr. Rupinder Basi	- Senior Planner
Mr. Mike Watson	- Planner
Mr. John Stark	- Acting Manager of Planning
Ms. Heather Corbett	- Committee Clerk

GUESTS:

Hossein Amanat, Bosa Developments
Dan Diebolt, Bosa Developments
Lindi Liebrandt, Bosa Developments

The meeting was called to order at 12:02 p.m.

ADDITIONS / DELETIONS TO THE AGENDA

There were no additions.

ADOPTION OF MINUTES

- 1. No Items

PRESENTATIONS

- 2. No Items

UNFINISHED BUSINESS

3. No Items

REPORTS FOR ACTION

4. **1111 Sixth Avenue (Former Church Site): Official Community Plan Amendment and Heritage Revitalization Agreement – Preliminary Report**

Mike Watson, Planner, summarized the preliminary report dated May 15 regarding an Official Community Plan (OCP) amendment application and a Heritage Revitalization Agreement (HRA) application for 1111 Sixth Avenue.

Discussion ensued and the Committee noted the following comments:

- While there may be advantages, the Committee expressed concerns about amending the draft OCP to accommodate this application;
- The application for a HRA is appropriate in this case; and,
- While there has been heritage restoration to the building in the past, it would be appropriate to include a high-level maintenance plan and ensure that structural deficiencies are examined in conjunction with this application.

MOVED and SECONDED

THAT the Land Use and Planning Committee recommend that Council direct staff to process the Heritage Revitalization Agreement and Development Permit applications based on the process outlined in the May 15 report; and, THAT this development goes through the OCP process independently.

CARRIED.

All members of the Committee present voted in favour of the motion.

5. **600 – 720 Quayside Drive (Bosa Site Formerly the Larco Site): Development Variance Permit – Preliminary Report**

Rupinder Basi, Senior Planner, summarized the report dated May 15, 2017, regarding a Special Development Permit (SDP) and a Development Variance Permit (DVP) application for the development of a mixed use project with two high rises and one three-storey commercial building at 600 to 720 Quayside Drive.

Hossein Amanat, Bosa Developments, provided a PowerPoint presentation summarizing details of the project and variance requested, as outlined in the report and attachments dated May 15, 2017.

In response to questions from the Committee, Mr. Basi, Mr. Watson, Mr. Amanat, and other Bosa representatives provided the following information:

- An environmental assessment of the waterfront area is required by other levels of Government and would have to be addressed by the applicant; and,
- Discussions are still ongoing with the Vancouver Fraser Port Authority about the proposed waterfront esplanade;

Discussion ensued, and the Committee noted the following comments:

- Concern was expressed regarding the construction phasing and the length of time that Begbie Street would be closed;
- Begbie Street is a prominent entrance which should include a high quality of urban design;
- This area has a rich industrial heritage and it should be recognized and celebrated;
- Appreciation was expressed for the clarification of the space between the two towers; and,
- Generally the proposed site plan revisions are positive and seem to have been well received by the public and stakeholders.

MOVED and SECONDED

THAT the Land Use and Planning Committee recommend that Council consider issuing notice for an Opportunity to be Heard at the Regular Meeting of Council on June 26, 2017 for DVP00617 varying CD-55 Zoning Bylaw requirements for 660 Quayside Drive as follows:

- *Up to 23 of the required motor vehicle parking spaces may be provided at-grade rather than within below-grade parking structures;*
- *Building footprint above the second storey in Sub-area 2 and Sub-area 3 may be up to 8,950 square feet rather than being limited to 6,500 and 7,500 square feet respectively;*
- *A revised site plan incorporating revised building siting within each Sub-area; and*
- *Revised building height limits as follows:*
 - *Sub-area 1: 23m (3 storeys) rather than 128 m (38 storeys)*
 - *Sub-area 2: 178m (53 storeys) rather than 156 m (47 storeys)*
 - *Sub-area 3: 144m (43 storeys) rather than 103 m (29 storeys)*

CARRIED.

All members of the Committee present voted in favour of the motion.

6. 630 Ewen Avenue (Affordable Housing Project): Official Community Plan and Rezoning Bylaws for First and Second Readings

Jim Hurst, Planner, summarized the report dated May 15, 2017, regarding an Official Community Plan Amendment and Rezoning Application for the development of a five unit residential building at 630 Ewen Avenue.

Mr. Hurst noted that the application has been supported by the Community and Social Issues Committee, the New Westminster Design Panel, and the Advisory Planning Commission and it satisfies numerous City policies and objectives.

MOVED and SECONDED

THAT the Land Use and Planning Committee recommend that Council consider Official Community Plan Amendment Bylaw 7919, 2017 for First and Second Reading and forward the Bylaw to a Public Hearing on June 26, 2017, and,

THAT the Land Use and Planning Committee recommend that Council consider Zoning Amendment Bylaw 7920, 2017 for First and Second Reading and forward the Bylaw to a Public Hearing on June 26, 2017.

CARRIED.

All members of the Committee present voted in favour of the motion.

7. 300 Salter Street (Port Royal): Development Permit and Development Variance Permit Applications for a Proposed 87 Unit Apartment Building

Jim Hurst, Planner, summarized the report dated May 15, 2017, regarding a Development Permit and Development Variance Permit Application for a proposed 87 unit apartment building at 300 Salter Street.

In response to questions from the Committee, Mr. Hurst provided the following information:

- The variance refers to eight units on the top floor which have a mezzanine level. The Mezzanines add to the height of the building; and,
- Permeability to the waterfront trail will occur once the third project in the development is complete.

MOVED and SECONDED

THAT the Land Use and Planning Committee recommend that Council forward Development Permit DPQ 00110 for 300 Salter Street to the Regular Meeting of Council on June 26, 2017 for consideration of issuance, and,

THAT the Land Use and Planning Committee recommend that Council issue notice that it will consider a motion to approve and issue Development Variance Permit

00612 to vary the height requirement at 320 Salter Street and forward the Development Variance Permit to an Opportunity to be Heard on June 26, 2017.

CARRIED.

All members of the Committee present voted in favour of the motion.

DIRECTOR'S / MANAGER'S REPORT (Oral Report)

8. No Reports

NEW BUSINESS

9. No Items

CORRESPONDENCE

10. No Items

ADJOURNMENT

ON MOTION, the meeting was adjourned at 12:30 p.m.

JONATHAN COTÉ
MAYOR

HEATHER CORBETT
COMMITTEE CLERK



REPORT

Development Services

To: Land Use and Planning Committee **Date:** 6/12/2017

From: Jackie Teed **File:** REZ00140
 Acting Director of Development SDP00212
 Services

Item #: 32/2017

Subject: 800 Columbia Street: Rezoning and Special Development Permit to Allow Liquor Primary Licensed Premise - Preliminary Report

RECOMMENDATION

THAT the Land Use and Planning Committee provide staff with feedback on the items outlined in Section 9 of this report;

THAT the Land Use and Planning Committee recommend that Council direct staff to process the Rezoning, Special Development Permit and Liquor Primary License applications based on the process outlined in this report;

EXECUTIVE SUMMARY

Applications have been received to amend the Zoning Bylaw and for a Special Development Permit to allow a Liquor Primary Licensed Premise at 800 Columbia Street in order to accommodate a Carlos O'Bryan's franchise pub within the existing 1898 Canadian Pacific Railway (CPR) station on site. The CPR Station has high heritage value locally and nationally for its historic, cultural and aesthetic significance. It is not formally protected but in 2004 was listed on the City, Provincial and National Heritage Registers

The applicant would like to lease the property from the current owner and would complete interior renovations to the existing building. The proposed liquor primary would be located on the upper floor and the lower floor would be used as a restaurant (called Kelly O'Bryan's). Exterior changes would be limited to site upgrades (landscaping and

landscaping) with the exterior of the heritage building remaining untouched, and staff would work with the applicant to ensure proposed exterior changes are compatible with the existing building and well integrated with surrounding sites including Hyack Square.

1. PURPOSE

These applications would all allow a new restaurant and pub at 800 Columbia Street. The purpose of this report is to request feedback from the Land Use and Planning Committee and request Council direction to proceed with the application review as outlined in this report.

2. BACKGROUND

2.1 Site Characteristics and Context:

The subject site is located within the downtown on the south-east corner of Columbia and Eighth Streets. The site currently includes one existing building which was constructed in 1898 to as station for the CPR railway. Previously this building was used for the Keg restaurant, but has been vacant since 2013. The subject site is relatively flat and currently has vehicle accesses to parking from Columbia Street and from Front Street at the rear.

Directly to the south of the site are Front Street and active rail lines. Beyond the rail lines are Quayside Drive and the River Market. To the east of the site is Hyack Square which is fronted by the restored Inter Urban Heritage building, currently the location of the Salvation Army Thrift Shop. To the north east of the site is the Anvil Centre. To the north of the site across Columbia Street is a vacant site currently owned by the City. To the north east of the site are the Shops at New West and three high rise residential towers.

The subject site currently consists of five different parcels for a total site area of 34,848 square feet and a density of 0.20 FSR. There is an existing right-of-way for Front Street and the railway corridor that crosses four of the parcels, thus reducing the buildable area to approximately 16,780 square feet. Based on buildable square footage, the current FSR is 0.4 FSR.

3. PROJECT DESCRIPTION

3.1 Project Description

The applicant has proposed to open a restaurant and a Liquor Primary Licensed Premises (LPLP), or pub, on the subject site at 800 Columbia Street within the historic CPR Station building. Applications for a Zoning Amendment Bylaw as well as a Special Development Permit have been received in order to permit the LPLP and the changes to the proposed site landscaping.

The applicant, Carlos O'Bryan's Pub and Kelly O'Bryan's Restaurant, would be leasing the building from the site owner. A Zoning Bylaw text amendment is necessary in order to allow the pub (LPLP) as a permitted use. LPLP uses are carefully regulated in New Westminster as there is the potential for impacts on surrounding uses.

Changes are also proposed to the site in terms of updated landscaping in order to improve the exterior patio space in front of the building including the provision of a covered area. There would, however, be no changes to the exterior of the existing heritage building other than the addition of new signage.

4. POLICY AND REGULATIONS

4.1 Policy/ Practice Regarding Liquor Applications

4.1.1 Participation in Liquor Control and Licensing Branch Comment Process

The Liquor Control and Licensing Branch's (LCLB) process for considering an application for a new Liquor Primary License requires that the applicant forward the Liquor Primary License application to the local municipality, which can determine whether or not they intend to submit comment on the application to the LCLB.

Should the local municipality choose to provide comment, they are required to seek public comments regarding the application and to have Council provide a resolution regarding the Liquor Primary License. This process has to be completed within 90 days of the local municipality confirming receipt of the Liquor Primary License application.

The City's practice is to participate in this process. The City requires applicants to post a sign on the site (similar to rezoning sign) and to post notices in two consecutive issues of the local newspaper. The City's practice is also to obtain comments from the Police Service, Liquor Inspector and City departments regarding the applicant's compliance record (if applicable), bylaw issues and potential community impacts. Comments received from these parties and the public are provided to Council for their use in consideration of a resolution to support or not support an application.

4.1.2 City Policy on Businesses Offering Liquor Services Downtown

In May 2010 Council endorsed principles included in a staff report regarding operation of businesses offering liquor services in the downtown. These principles support: 1) operating hours of restaurants, neighbourhood pubs and similar businesses to 2 am; 2) private clubs to 1 am; and 3) no extension of operating hours for existing nightclubs or cabarets, including no liquor premise shall be operating later than 2 am.

Further, the policy states that new liquor services in the downtown should only be considered that are niche type establishments which are upscale and contribute to the character and vitality of the area. Establishments of formats such as cabarets, nightclubs or strip clubs should not be considered.

4.2 OCP Land Use Designation

The proposal is consistent with the current Columbia Street Historic Mixed-Use designation which is described as:

- mixed-use (commercial and/or residential) along Columbia Street;
- retail, office, service, restaurant, entertainment, arts and culture, recreation at street level;
- identified heritage sites will be protected;
- all development on non-heritage sites must respect adjacent heritage
- façade retention will be encouraged over full
- demolition of a heritage building

4.3 Downtown Community Plan

The subject site is located within the Columbia Street Character Precinct within the Downtown Community Plan which describes the intent as: to provide protection, incentives and opportunities to respect, enhance and celebrate the recognized heritage resources within it. The Historic Precinct is identified as Downtown's main commercial corridor, a role it has played since the 1860s.

4.4 Zoning Bylaw

The subject site is currently zoned Columbia Street Historic Comprehensive Development Districts (C-8). The current zoning does not permit Liquor Primary Licensed Premises. The Zoning Bylaw only allows Liquor Primary Licensed Premises in locations which are listed in Appendix H of the Zoning Bylaw.

4.5 Development Permit Area

The site is within the #1 Downtown Development and Special Development Permit Area. The proposal meets the intent of the Development Permit Area, which seeks to support the Downtown's Regional Town Centre designation in the Regional Growth Strategy. This Development Permit Area outlines objectives and guidelines for:

- The form and character of commercial, multifamily, institutional and intensive residential development.
- Protection of the natural environment, its ecosystems and biological diversity.

- Revitalization of an area in which a commercial use is permitted.
- Objectives to promote energy and water conservation and reduction of greenhouse gas emissions.

In this area, Special Development Permits will be issued as authorized by the New Westminster Redevelopment Act.

4.6 Standards and Guidelines for the Conservation of Historic Places in Canada

Council adopted the “Standards and Guidelines for the Conservation of Historic Places in Canada” (“Standards & Guidelines”) in 2008 as a basis for assessing heritage projects within the city. Although a formal Heritage Application is not required for this project, it will still be carefully evaluated the guidelines by staff to confirm compliance.

5. DISCUSSION

5.1 Heritage Value of Existing Building

The former Canadian Pacific Railway (CPR) station has high heritage value locally and nationally for its historic, cultural and aesthetic significance. It is not formally protected, but in 2004 was listed on the City, Provincial and National Heritage Registers.

The station was constructed in 1898/99 to replace the original wooden 1887 station that was destroyed in the Great Fire of 1898. It was designed by the CPR’s chief architects, Edward Maxwell and his brother William, using the CPR’s signature Chateau style. This style, consistently employed for the company's stations and hotels, is characterized by steeply pitched roofs, tall chimneys and picturesque silhouette. The Maxwells were among the most successful and accomplished architects in Canada in the early decades of the Twentieth Century.

The station remains as a symbol of the importance of rail transport for movement of people and goods in the formative years of New Westminster and the province.

The location of this building, adjacent to the British Columbia Electric Railway station and the working waterfront of the Fraser River, demonstrates the historic role of New Westminster as a regional transportation nexus. The building itself represents the union of British Columbia with the Dominion of Canada as it is the site of the first branch line into British Columbia.

The building was enlarged in 1911 with the addition of two wings. These were designed by the local, well-known architectural team of Gardiner and Mercer. Other than some interior alterations to create offices in 1972, the CPR appears to have made very few alterations during the time that the building was in use as a station.

The station is in a key location that, combined with its scale and unique architecture, marks the entrance to the city's historic downtown. The station has iconic status locally and nationally and is a strong candidate for formal heritage protection. The Statement of Significance is attached as Appendix A.

The proposed restaurant and liquor primary licensed premises uses would not require any additions to the building or changes which would impact the character defining elements of the historic building.

5.2 Protection of Existing Building

Given, the historic significance of the building, the proposed rezoning to allow a Liquor Primary Licensed Premise offers an opportunity to discuss the opportunity for a Heritage Designation with the property owner while continuing to proceed with the review of the applicant / tenant's application for a Liquor Primary Licensed Premise.

A typical concern of property owners regarding Heritage Designation would be the potential loss of development entitlements. However, a Heritage Designation at this time would not preclude the opportunity for a future Heritage Revitalization Agreement which would allow the owner to sell and transfer the residential density from the existing site to another development site in the downtown through the Heritage Density Transfer Program.

5.3 Relationship to Hyack Square

The application proposes updates to the exterior patio space in front of the building and adjacent to Hyack Square. Currently, the application includes the addition of a large gazebo / covered space and the delineation of the proposed outdoor seating area with low level fencing.

The proposed changes present an opportunity to work with the applicant to upgrade some of the surface treatment in front of the building and to integrate the space more seamlessly with Hyack Square. Hyack Square is currently surfaced with concrete scored in simple linear pattern (see photograph #1 below) which is similar to the treatment used in front of the Anvil Centre and being explored in association with the development across Columbia Street at the north-east corner of this intersection. The surface treatment in front of the building is currently red brick material laid in spiral pattern as (see the photograph #2 below). This pattern or material does not have any historic connection and was likely added to the site in the 1980s. This material is on both private property and on the public sidewalk. Many of the bricks are heaving or sinking, creating a very uneven surface. There would be several advantages to upgrading the surface treatment including:

- highlighting and make the heritage building more prominent;
- making the space feel larger and more contiguous with Hyack square;

- improving universal accessibility and eliminating tripping hazards;
- simplifying materials and ensuring consistency with other developments at this intersection; and
- reducing maintenance costs for both the applicant/owner and the City.

Staff would discuss updating the surface treatment around the building with the owner and the tenant to improve the aesthetics and function of both the privately owned space and the City sidewalk.



Photograph #1 – Hyack Square



Photograph #2 – CPR Station Building

5.4 Proposed Use

Liquor Primary Licensed Premises (LPLP) are currently a carefully regulated use within the Zoning Bylaw and only permitted on a site specific basis. Any new LPLP needs to under-go a rezoning application review process which offers the opportunity for both public consultation and Council evaluation of all applications. The City has been seeking establishments that are upscale and which contribute to the unique character and vitality of the downtown and has been actively discouraging formats such as cabarets, nightclubs and strip clubs.

The proposed LPLP on the subject site requires that the Zoning Bylaw be amended to include the site as well as the number of liquor seats in the premises, within Appendix H. The applicant proposes that the LPLP include 83 liquor seats and be permitted to operate from 9 am to 2 am Monday through Saturday and 9 am to midnight on Sundays.

City policy is to consider operating hours for restaurants, neighbourhood pubs and similar businesses to 2 am; private clubs to 1 am; and no extension of operating hours for existing nightclubs or cabarets, including that no liquor premise shall operate later than 2am.

The table below provides a summary of the other LPLPs currently in the Downtown including the number of seats and the business closing time.

Business	Liquor Seats	Closing Time
57 Below (Hops a Northwest Pub)	140 seats	12:00 am on weekdays and 1:00 am Friday & Saturday
Brooklyn Bar and Grill	150 seats	12:00 am
Fever Night Club (Club Magnetique)	350 seats	2:00 am
Inn at New Westminster Quay	100 seats	1:00 am
Lafflines at the Burr	110 seats	2:00 am
Old Terminal Pub	145 seats	12:00 am on weekdays and 1:00 am Friday & Saturday
Paddlewheeler Restaurant and Pub	184 seats	1:00 am
The Metropolitan	90 seats	1:00 am on weekdays and 2:00 am Friday & Saturday
Anvil Centre	1785 seats	12:00 am on weekdays and 1:00 am Friday & Saturday

Judge Begbie's Tavern	76 seats	12:00 am on weekdays and 1:00 am Friday & Saturday
Elk's Club of New Westminster	150 seats	11:00 pm

5.5 Process in Relation Provincial Liquor Primary License Requirement

The required zoning amendment and the Provincial Liquor Primary License applications can be process in parallel. This would reduce the necessary processing time for the applicant and allow for public feedback for the Liquor Primary License and the rezoning to be received at the same time. Section 7 below outlines this process.

5.6 Parking and Access

The subject site currently provides six off-street parking spaces and is lawfully non-conforming in that regard. The applicant has proposed to include seven parking spaces on the site. Adding a parking space would reduce the existing lawful non-conformity and would be permitted.

Currently the site includes one vehicle crossing from Columbia Street and two vehicles crossings from Front Street. The applicant has proposed to eliminate the crossing on Columbia Street and use only the crossings on Front Street.

6. FEEDBACK FROM LAND USE AND PLANNING COMMITTEE

During staff's review of the preliminary plans several issues have been identified where comments and consideration from LUPC are sought.

1. Heritage Designation: Is the LUPC supportive of staff discussing the opportunity for a Heritage Designation on the subject site and to potentially be an adoption requirement of the applicant / tenant's zoning amendment bylaw to permit a Liquor Primary Licensed Premise.
2. Requested Operating Hours: Is the LUPC supportive of the operating hours of 9am to 2am Monday through Saturday and 9am to midnight on Sundays, proposed by the applicant.

7. CONSULTATION

The applicant would be required to undertake public engagement both as part of the rezoning application and the application for a Liquor Primary License. Consultation would include a presentation to the Downtown Residents' Association and an applicant led Public Open

House in addition to notice for surrounding property owners, newspaper postings and application signage on the site.

8. REVIEW PROCESS

The anticipated steps in the rezoning and Liquor Primary License application review process are:

1. Preliminary Report to Land Use and Planning Committee (June 12, 2017);
2. Land Use and Planning Committee referral of Applications to Council. (July 10, 2017);
3. Review by the Community Heritage Commission;
4. Applicant-led Public Open House;
5. Presentation to the Downtown Residents Association;
6. Initiation of Public Consultation Period for Liquor Primary License;
7. Review by the Advisory Planning Commission;
8. Land Use and Planning Committee Consideration of Referral of Applications to Council for Consideration of First and Second Reading of Zoning Amendment Bylaw;
9. Council Consideration of First and Second Reading of Zoning Amendment Bylaw;
10. Public Hearing and Council Consideration of Third Reading of Zoning Amendment Bylaw;
11. Council Consideration of Resolution Regarding Liquor Primary License for Liquor Control and Licensing Branch;
12. Adoption of Zoning Amendment Bylaw;

9. INTERDEPARTMENTAL LIAISON

The City has now initiated a project team based approach for reviewing development applications. A staff-led project team has been assigned for reviewing this project consisting of staff from the Building, Planning and Licensing and Integrated Services (Development Services), Engineering and Parks and Recreation. Other Departments will also be included early in the process as needed.

10. OPTIONS

The following options are offered for consideration of the LUPC:

1. That the Land Use and Planning Committee provide staff with feedback on the items outlined in Section 9 of this report;

2. That the Land Use and Planning Committee recommend that Council direct staff to process the Rezoning, Special Development Permit and Liquor Primary License applications based on the process outlined in this report;
3. That the Land Use and Planning Committee provide staff with alternative feedback.

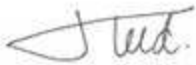
Staff recommends options 1 and 2.

ATTACHMENTS

Appendix A: 800 Columbia St Statement of Significance

This report has been prepared by:
Mike Watson, Planner

This report was reviewed by:
John Stark, Acting Manager of Planning



Jackie Teed
Acting Director of Development
Services



Appendix A

800 Columbia Street Statement of Significance

Canada's
Historic PlacesLieux patrimoniaux
du Canada

Canadian Pacific Railway Station

[Return to listing](#)

Canadian Pacific Railway Station

800 Columbia Street, New Westminster, British Columbia

Other Name(s)

CPR Station

Links

n/a

Image(s)

[oblique view](#)[front elevation](#)[side view](#)

Statement of Significance

Description of Historic Place

The Canadian Pacific Railway (CPR) Station is a late Victorian two-storey asymmetrical red brick and sandstone clad building with steeply-pitched hipped bellcast roofs. It is situated on a corner lot adjacent to the former BCER (B.C. Electric Railway) Station, on the south side of Columbia Street, the main commercial street in New Westminster's historic downtown core. The site has additional frontages at the rear facing Front Street, and at the side on Eighth Street, which has now been closed and paved as a plaza. It is a landmark structure that is visible on all sides and marks the western point of entry to the downtown area.

Heritage Value

The Canadian Pacific Railway Station is of architectural significance as a fine example of the CPR's signature Chateau style. This style was consistently employed for the company's stations and hotels and was characterized by steeply pitched roofs, tall chimneys and picturesque silhouette. The location of this building, adjacent to the British Columbia Electric Railway station and the working waterfront of the Fraser River, demonstrates the historic role of New Westminster as a regional transportation nexus. The station was designed by architect Edward Maxwell (1867-1923). Maxwell, and his brother William S., with whom he formed a partnership in 1902, were the only Canadian-born architects employed regularly by the CPR. In British Columbia, Edward Maxwell designed additions to the CPR's Glacier Hotel, a hotel and station at Sicamous Junction as well as this station in New Westminster. The Maxwells were among the most successful and accomplished architects in Canada in the early decades of the twentieth century, and in addition to their numerous works for the CPR, they undertook major commissions for corporate clients such as Henry Birks and Sons, and the Bell Telephone Company, and institutional projects such as the Saskatchewan Legislative Building. As the population increased, the station was enlarged in 1911 with two flanking wings, this time designed by prominent local architects Gardiner and Mercer.

The Canadian Pacific Railway Station is directly associated with the development of downtown New Westminster. In 1886, the CPR extended a spur line to New Westminster, sparking a boom in local development. After the devastation of the Great Fire of 1898, the downtown was rebuilt immediately and continued to develop during the great western Canadian economic boom that preceded the First World War. After the Fire destroyed their original 1887 wooden station and sheds, the CPR immediately rebuilt in 1899, this time in fireproof materials. The Canadian Pacific Railway Station remains as a symbol of the importance of rail transport for movement of people and goods in the formative years of New Westminster and the province. Prior to the current highway network that connects the province, the railways of the CPR and other companies offered the primary method of transportation over land for medium and long distances.

Source: Heritage Planning Files, City of New Westminster

Character-Defining Elements

- Key elements that define the heritage character of the Canadian Pacific Railway Station include its:
- location with frontages on Columbia Street, Front Street, and Eighth Street (which has now been closed and paved as a plaza), part of a grouping of late Victorian and Edwardian era commercial buildings in historic downtown New Westminster
 - spatial relationship to the BCER Station across Eighth Street
 - asymmetrical, picturesque form, two-storey scale, and cubic massing, double-height gable at front entry with stone coping and brackets, and polygonal turrets facing the original track side of the building
 - steeply-pitched bellcast hipped roofs, including polygonal tower roofs
 - masonry construction and detailing, including red brick walls and rough-dressed sandstone foundation and trim
 - exterior detailing such as corbelled brick and stone course at the roof-line, stone quoining and window surrounds
 - original fenestration, including wooden-sash casement windows with transoms
 - original interior features such as wooden wainscoting and wooden trim

Location

Province/Territory

British Columbia

Street Address

800 Columbia Street

Location

Greater Vancouver Regional District
Burnaby
New Westminster

Recognition

Jurisdiction

British Columbia

Recognition Authority

Local Governments (BC)

Recognition Statute

Local Government Act, s.954

Recognition Type

Community Heritage Register

Recognition Date

4/5/2004

Historical Information

Construction Date(s)

1899

Significant Date(s)

1910 to 1911

Theme - Category and Type

Developing Economies
Communications and Transportation

Function - Category and Type

Historic

Transport-Rail
Station or Other Rail Facility

Current

n/a

Architect / Designer

Gardiner and Mercer
Edward Maxwell

Builder

n/a

Additional Information

Related Listing(s)

n/a

Location of Supporting Documentation

Heritage Planning Files, City of New Westminster

Cross-Reference to Collection

n/a

Fed/Prov/Terr Identifier

DhRr-159

Status

Listed

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Last updated: 24/08/2010

[Page Top](#)

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REPORT

Development Services

To: Land Use and Planning Committee **Date:** 6/12/2017

From: Jackie Teed **File:** DPQ00125
Acting Director of Development
Services

Item #: 33/2017

Subject: 1050 Boyd Street: Development Permit Application - Preliminary Report

RECOMMENDATION

THAT the Land Use and Planning Committee direct staff to process this application as outlined in this report.

1. PURPOSE

This applicant has applied for a Development Permit to allow construction of a new car dealership on the property. The purpose of this report is to seek a resolution from Land Use and Planning Committee to process this application.

2. POLICY AND REGULATIONS

2.1 Official Community Plan Land Use Designation

The site is designated **(LI) – Light Industrial** in the Official Community Plan. The Plan describes this designation as:

(LI) Light Industrial – This area will include light industrial uses.

2.2 Official Community Plan Development Permit Area Designation

The site is designated as part of Industrial and Mixed Employment Development Permit Area #2 **Queensborough Light Industrial and Mixed Employment**. The purpose of this Development Permit Area is:

The Queensborough Light Industrial and Mixed Employment industrial areas, identified as Development Permit Area #2 [see Map C], are designated in order to provide areas of light and “ultralight” industrial and commercial land use as a transition between heavier industrial areas and residential areas.

This site is also designated as part of **Natural Hazards Development Permit Area # 1 – Flood Hazard**. The purpose of this Development Permit area is to provide:

Guidelines for this development permit area are intended to minimize the potential for loss of life and property damage in the event of flooding of the Fraser River, while allowing for the continued use of industrial lands to provide employment and the continued renewal and development of an historic New Westminster neighbourhood.

The Development Permit Area does not set standards for commercial uses. The sale and repair of new and used vehicles is a commercial use. The applicants have chosen to raise the building to satisfy the minimum geodetic elevation identified in the Development Permit Area for other buildings in Queensborough.

2.3 Zoning Bylaw

The site is zoned Light Industrial Districts (M-1). The sale and repair of new and used automobiles is an allowed use in the zone. The proposed development conforms to the land use and density permitted in the Light Industrial Districts (M-1) zone.

3.0 BACKGROUND

3.1 Site Characteristics and Context

The site is currently surrounded on all sides by road, with no adjacent neighbours. To the north is the Queensborough Landing Shopping Centre. To the east are some light industrial uses. To the south is the Queensborough residential area.

3.2 Project Description

The applicant proposes to construct a new building for a Toyota dealership with 37,519.5 square feet (3,485.7 square metres) of total floor space. The building is two levels with sales on the ground floor and additional office and employee areas on the second level. The

building is designed to provide 88 screened vehicle parking spaces on the roof. The roof is accessed by a ramp at the east end of the building.

Project Statistics:

Site Area:	97,198 square feet (9,030 square metres)
Zoning:	Light Industrial Districts (M-1)
Proposed Floor Space Ratio:	0.39
Proposed Site coverage:	31.9 %
Proposed Height:	29.5 feet (9.0 meters)

4.0 DISCUSSION

4.1 Access, Site Design and Streetscape

The site has a number of restrictions that limit access to the site. The south edge of the site is the on ramp to the highway so no site access is possible. The east edge of the site is Howes Street. Access in this area is controlled by the Ministry of Transportation and Infrastructure who will not approve any site access in this corridor. The north edge of the site on Boyd Street has a right turn lane and a bicycle lane adjacent to the site which forces site access to a location on Boyd Street at the furthest western point of the site.

The applicant has requested that left turns be permitted from Boyd Street. This can be considered within the current road dedication. The site plan shows an extension of the multi-use pathway along the Howes Street frontage. The established multi-purpose pathway continues on the north side of Boyd Street, so no formal sidewalk or pathway is identified for the Boyd Street frontage.

A link between the bus stop on the highway on ramp, the multi-use pathway on Howes Street and a stairway up to the Toyota site are part of the plan. Staff is currently reviewing the bus stop location and will move the stop toward Howes Street if possible.

Staff will review the provision of street trees and the location of the curb line and multi – purpose pathway during detailed design review.

4.2 Urban Design and Flood Plain Considerations

The proposal is a car dealership which is seeking to have as much visibility to the site as possible. The grades on the site are being raised so that the building is above the flood plain elevation. This gives the building a prominence while satisfying flood plain regulations. The

raising of the building does give the site a separation from the adjacent streets. During design review staff will look at the opportunities to further link the site and the street.

At the Howes Street and Boyd Street corner of the site, the design is based on a riparian assessment of the area. During preloading of the Toyota site it was identified that the riparian area existed due to failed drainage in the area. A reassessment of this area is currently being conducted and an alternate design may be part of the final proposal. This could provide an opportunity to link the site directly to the corner of Howes and Boyd Streets and provide a small green amenity at street level.

5.0 PROCESS

The next steps in the application review process are;

1. LUPC recommendation to initiate the processing of this application.
2. Application is reviewed by all City Departments.
3. The project is considered by the New Westminster Design Panel.
4. The applicant will provide information to the Queensborough Residents' Association.
5. Council consideration of the issuance of the Development Permit.

6.0 OPTIONS

There are two options for consideration; they are:

1. That the Land Use and Planning Committee direct staff to process this application as outlined in this report.
2. That the Land Use and Planning Committee provide staff with alternative feedback.

Staff recommends Option 1.

ATTACHMENTS

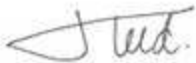
Attachment 1: Location Map

Attachment 2: Project Plans

Attachment 3: DP Area Guidelines

This report has been prepared by:
Jim Hurst, Planner

This report was reviewed by:
John Stark, Acting Manager of Planning




Jackie Teed
Acting Director of Development
Services

Attachment 1
Location Map



0.1 0 0.06 0.1 Kilometers

NAD_1983_UTM_Zone_10N
CNW GIS Services

1:2,500 

This map is a user generated static output from an Internet mapping site and is for reference only. Data layers that appear on this map may or may not be accurate, current, or otherwise reliable.

Attachment 2

Project Plans

THE DESIGN OF ALL SUPPLEMENTARY ARCHITECTURAL COMPONENTS INCLUDING BUILDINGS, CEILING, EQUIPMENT, FLOORING AND OTHER BUSINESS ITEMS MUST BE SUBMITTED TO THE ARCHITECT FOR REVIEW AND APPROVAL. THE ARCHITECT'S REVIEW AND APPROVAL DOES NOT CONSTITUTE A GUARANTEE OF THE ACCURACY OF THE INFORMATION PROVIDED OR THE DESIGN OF THE SUPPLEMENTARY ARCHITECTURAL COMPONENTS. THE ARCHITECT'S REVIEW AND APPROVAL IS LIMITED TO THE DESIGN OF THE SUPPLEMENTARY ARCHITECTURAL COMPONENTS AND DOES NOT CONSTITUTE A GUARANTEE OF THE ACCURACY OF THE INFORMATION PROVIDED OR THE DESIGN OF THE SUPPLEMENTARY ARCHITECTURAL COMPONENTS. THE ARCHITECT'S REVIEW AND APPROVAL IS LIMITED TO THE DESIGN OF THE SUPPLEMENTARY ARCHITECTURAL COMPONENTS AND DOES NOT CONSTITUTE A GUARANTEE OF THE ACCURACY OF THE INFORMATION PROVIDED OR THE DESIGN OF THE SUPPLEMENTARY ARCHITECTURAL COMPONENTS.

SHEET NOTES

- SITE LEGEND:**
ELEVATIONS ARE IN IMPERIAL UNITS
CURB TYPES:
 - CAST-IN PLACE CONC. CURB REFER TO CIVIL DWG. FOR DETAILS
 - EXTRUDED CONC. CURB
 - CONC. CURB & SIDEWALK
 - TRUCKING CURB
 - ALL CURBS PAINTED - MIN. 6" WIDE
 - ALL PARKING LOT LINE PAINTING TO BE TRAFFIC YELLOW.
- CATCH BASIN
 - EXISTING ELEVATION
 - PROPOSED NEW FIN. GRADE
 - FIRE HYDRANT
 - BIANISSE CONNECTION
 - MANHOLE OR RIM
 - TOP OF CONCRETE
 - STOP SIGN
 - ACCESSIBLE PARKING SIGN
 - BOLLARD
 - LAMP STANDARD
 - CAR PLUG-IN STATION
 - MAIN BUILDING ENTRANCE
 - SECONDARY BUILDING ENTRANCE
 - CONCRETE LETDOWN
 - BIKE RACK
 - PAINTED ACCESSIBLE PARKING SIGN
 - PAINTED "SMALL CAR ONLY"
 - LANDSCAPING
 - CONCRETE
 - TRANSFORMER VAULT (SEE ELEC. DWGS.)
 - PAINTED TRAFFIC ARROWS
 - PAINTED STOP LINE
 - PAINTED CROSSING
 - PROPERTY LINE
 - FIRE TRUCK ROUTE & HEAVY DUTY ASPHALT
- MAR. 09.2017 ISSUED FOR DP
- DEC. 23.2016 ISSUED FOR DP PREAPPLICATION REVIEW
- NOV. 26.2016 PRE-DP REVIEW MEETING
- NO. DATE ISSUE/ADDENDUM/REVISION

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SEAL:

SITE PLAN

PROJECT NO: 2610
DRAWN BY:
CHECKED BY:
DATE: 09/16/10
SCALE: As indicated
SHEET:

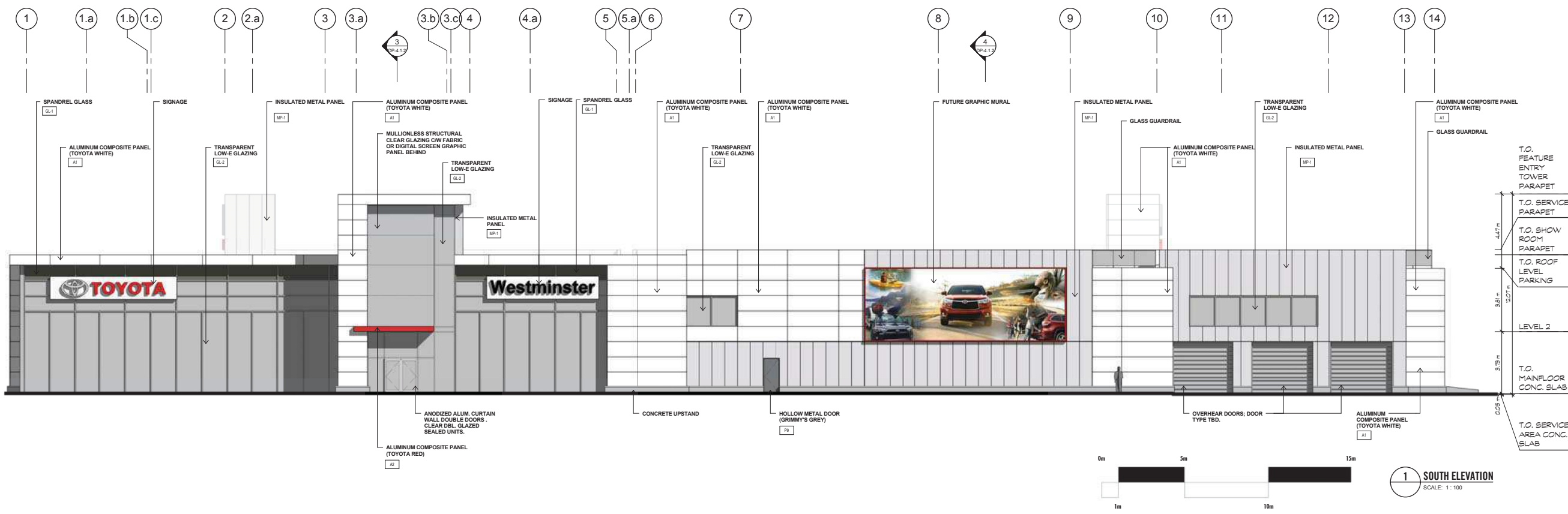
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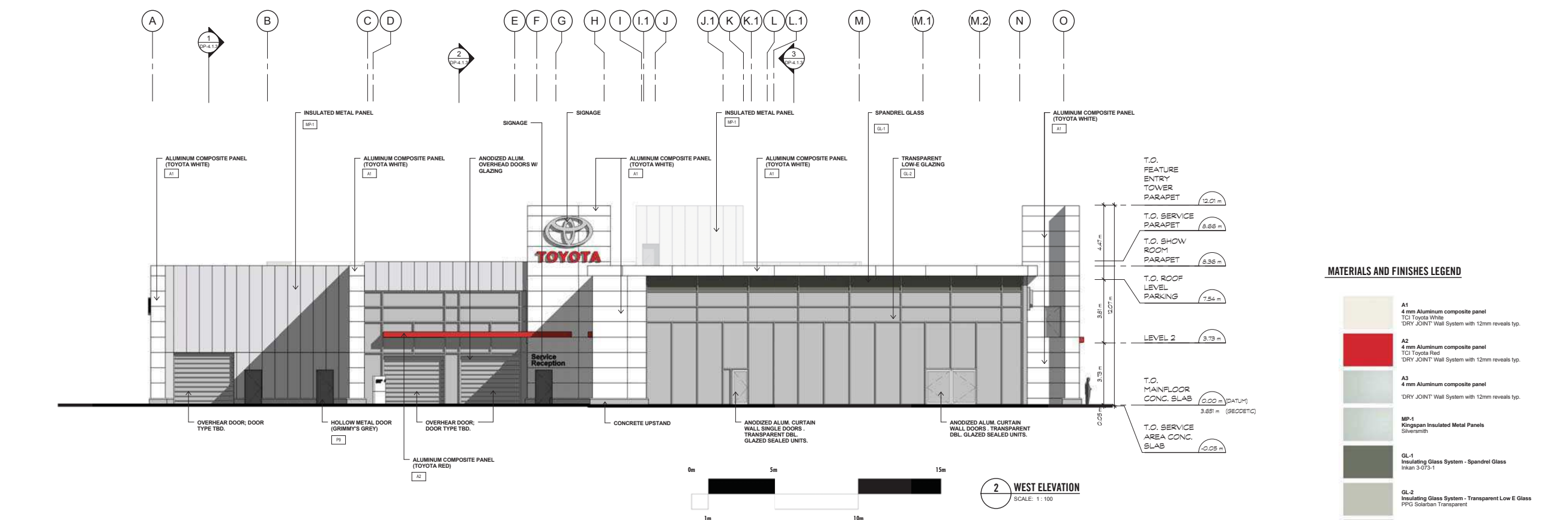
1 SITE PLAN
SCALE: 1:200

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THE DESIGN OF ALL SUSPENDED ARCHITECTURAL COMPONENTS INCLUDING BALUNES, CEILING EQUIPMENT, PIPING AND OTHER SUSPENDED ITEMS MUST BE CHECKED BY PROFESSIONAL ENGINEERS REGISTERED TO PRACTICE IN THE PROVINCE OF BRITISH COLUMBIA AND EMPLOYED DIRECTLY BY THE RESPECTIVE CONTRACTOR AND SUBCONTRACTOR. ALL DESIGN AND DESIGN LOAD REQUIREMENTS AS SET OUT IN THE CONTRACT SHALL BE WITH REFERENCE TO THE BRITISH COLUMBIA BUILDING CODE, LATEST EDITION. THE PROFESSIONAL STRUCTURAL ENGINEER SHALL PREPARE SIZES AND SEALS DRAWINGS AND CERTIFY THE COMPLIANCE WITH THE BRITISH COLUMBIA BUILDING CODE SCHEDULES B AND S.C. OF THE BRITISH COLUMBIA BUILDING CODE. THE ORIGINALS SHALL BE SUBJECT TO THE APPROVAL OF THE OWNER AND ARCHITECT WITH REGARD TO SUITABILITY OF APPEARANCE AND COMPATIBILITY WITH ADJACENT WORK.



1 SOUTH ELEVATION
SCALE: 1:100



2 WEST ELEVATION
SCALE: 1:100

MATERIALS AND FINISHES LEGEND

- A1 4 mm Aluminum composite panel
TCl Toyota White
DRY JOINT Wall System with 12mm reveals typ.
- A2 4 mm Aluminum composite panel
TCl Toyota Red
DRY JOINT Wall System with 12mm reveals typ.
- A3 4 mm Aluminum composite panel
DRY JOINT Wall System with 12mm reveals typ.
- MP-1 Kingspan Insulated Metal Panels
Silvermint
- GL-1 Insulating Glass System - Spandrel Glass
Inkan 3-073-1
- GL-2 Insulating Glass System - Transparent Low E Glass
PPG Solarban Transparent
- FR Exterior Door and Frames
Grimmy's Grey #09NN 201000
Acrylic Urethane Semi-Gloss

SHEET NOTES

- 1 -
 - 2 -
- | NO | DATE | ISSUE / ADDENDUM / REVISION |
|----|--------------|-------------------------------------|
| 4 | MAR. 09.2017 | ISSUED FOR DP |
| 3 | DEC. 23.2016 | ISSUED FOR DP PREAPPLICATION REVIEW |
| 2 | NOV. 28.2016 | PRE-SP REVIEW MEETING |
| 1 | NOV. 18.2016 | PRE-SP MEETING |

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SEAL:

BUILDING ELEVATIONS

PROJECT NO: 2610
DRAWN BY:
CHECKED BY:
DATE: 09/20/10
SCALE: As indicated
SHEET:

DP-4.0.1

THE DESIGN OF ALL SUSPENDED ARCHITECTURAL COMPONENTS INCLUDING BALCONIES, CEILING, EQUIPMENT, PIPING AND OTHER SUSPENDED ITEMS MUST BE PERFORMED BY A PROFESSIONAL STRUCTURAL ENGINEER, REGISTERED TO PRACTICE IN THE PROVINCE OF BRITISH COLUMBIA AND EMPLOYED DIRECTLY BY THE RESPECTIVE CONTRACTOR AND SUBCONTRACTOR. THE PROFESSIONAL STRUCTURAL ENGINEER SHALL PREPARE SCHEDULES AND SEALS KNOWING AND CERTIFYING THE COMPLETE INSTALLED BUILDING SCHEDULES S.B. AND S.C. OF THE BRITISH COLUMBIA BUILDING CODE THE DESIGN AND ALSO SUBJECT TO THE APPROVAL OF THE OWNER AND ARCHITECT WITH REGARD TO SAFETY OF APPEARANCE AND COMPATIBILITY WITH ADJACENT WORK.

SHEET NOTES

T.O. FEATURE ENTRY TOWER PARAPET	12.01 m
T.O. SERVICE PARAPET	8.66 m
T.O. SHOW ROOM PARAPET	8.36 m
T.O. ROOF LEVEL PARKING	7.54 m
LEVEL 2	3.73 m
T.O. MAIN FLOOR CONC. SLAB	0.00 m (D.A.T.U.M.)
T.O. SERVICE AREA CONC. SLAB	3.65 m (GEODETIC)
	-0.05 m

4	MAR. 09.2017	ISSUED FOR DP
3	DEC. 23.2016	ISSUED FOR DP PREAPPLICATION REVIEW
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1	NOV. 18.2016	PRE-OP MEETING
NO	DATE	ISSUE / ADDENDUM / REVISION

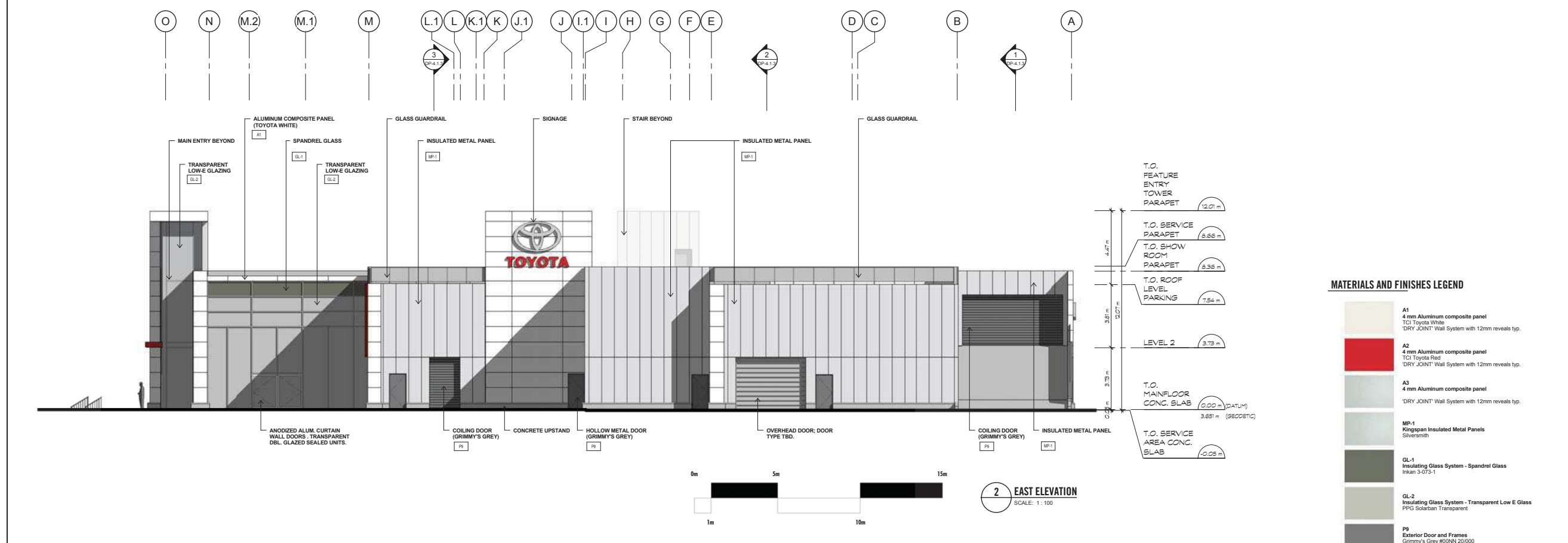
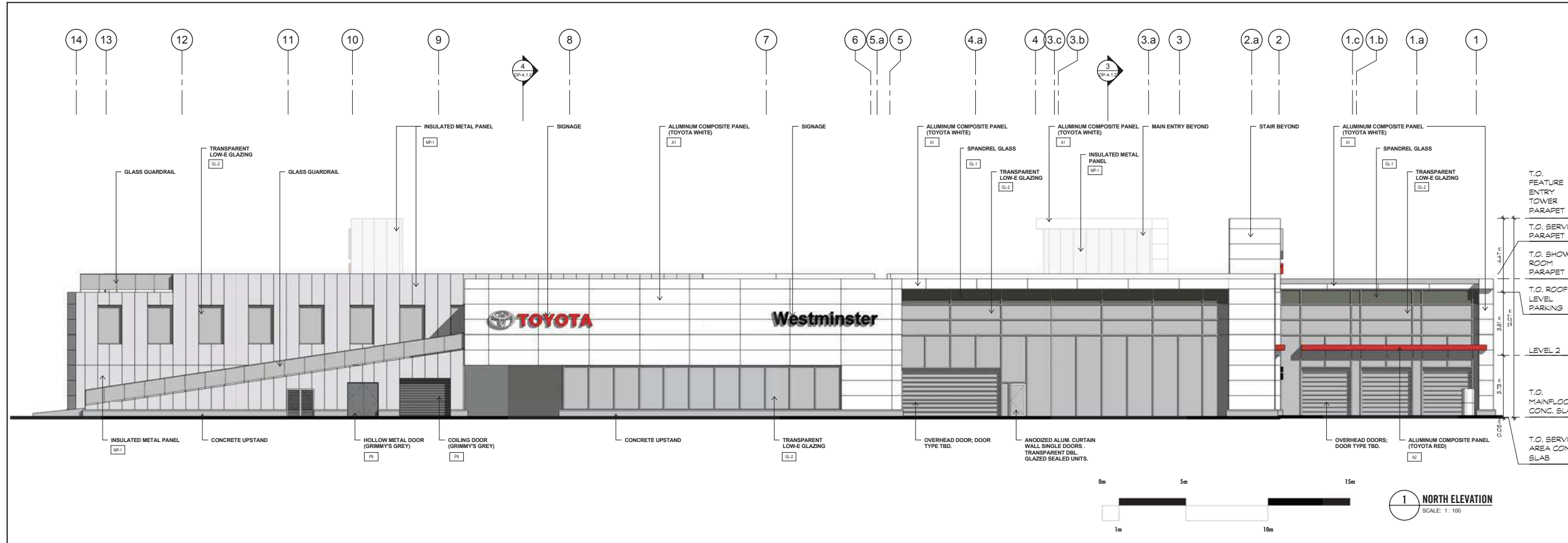
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BUILDING ELEVATIONS

SHEET TITLE:

PROJECT NO:	3810
DRAWN BY:	Author
CHECKED BY:	Checker
DATE:	10/28/16
SCALE:	As indicated

SHEET: **DP-4.1.1**



THE DESIGN OF ALL SUSPENDED ARCHITECTURAL COMPONENTS INCLUDING BALCONIES, CEILING, EQUIPMENT, PIPING AND OTHER SUSPENDED ITEMS MUST BE CHECKED BY PROFESSIONAL STRUCTURAL ENGINEERS, REGISTERED TO PRACTICE IN THE PROVINCE OF BRITISH COLUMBIA AND EMPLOYED DIRECTLY BY THE RESPECTIVE CONTRACTOR AND SUBCONTRACTOR. ENGINE SHALL TAKE INTO ACCOUNT ALL SEISMIC AND DESIGN LOAD REQUIREMENTS AS SET OUT IN THE BRITISH COLUMBIA BUILDING CODE, LATEST EDITION. THE PROFESSIONAL STRUCTURAL ENGINEER SHALL PREPARE SIGNED AND SEALED DRAWINGS AND CERTIFY THE COMPLIANCE WITH ALL BUILDING SCHEDULES B AND S.C. OF THE BRITISH COLUMBIA BUILDING CODE. THE ORIGINALS SHALL BE SUBJECT TO THE APPROVAL OF THE OWNER AND ARCHITECT WITH REGARD TO SUITABILITY OF APPEARANCE AND COMPATIBILITY WITH ADJACENT WORK.

SHEET NOTES



1 PERSPECTIVE - SOUTH-WEST
SCALE: 1:1



2 PERSPECTIVE - NORTH-WEST
SCALE: 1:1



3 PERSPECTIVE - NORTH-EAST
SCALE: 1:1



4 PERSPECTIVE - SOUTH-EAST
SCALE: 1:1

4	MAR. 09.2017	ISSUED FOR DP
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SEAL:

SHEET TITLE:
PERSPECTIVES

PROJECT NO:	2610
DRAWN BY:	
CHECKED BY:	
DATE:	11/10/16
SCALE:	1:1
SHEET:	DP-5.1.1

THE DESIGN OF ALL SUSPENDED ARCHITECTURAL COMPONENTS INCLUDING BALCONIES, CEILING, EQUIPMENT, PIPING AND OTHER SUSPENDED ITEMS MUST BE CHECKED BY PROFESSIONAL STRUCTURAL ENGINEERS, REGISTERED TO PRACTISE IN THE PROVINCE OF BRITISH COLUMBIA AND EMPLOYED DIRECTLY BY THE RESPECTIVE CONTRACTOR AND SUBCONTRACTOR. DESIGN SHALL COMPLY WITH STRUCTURAL, ASSESSMENT AND DESIGN LOAD REQUIREMENTS, AS SET OUT IN THE PROFESSIONAL STRUCTURAL ENGINEER'S DESIGN REQUIREMENTS AND DESIGN SCHEDULES. THE REGISTERED INSTALLATION SCHEDULES S.B AND S.C OF THE BRITISH COLUMBIA BUILDING CODE. THE ORIGINAL SHALL BE SUBJECT TO THE APPROVAL OF THE OWNER AND ARCHITECT WITH REGARD TO SUITABILITY OF APPEARANCE AND COMPATIBILITY WITH ADJACENT WORK.

SHEET NOTES

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WESTMINSTER TOYOTA DEALERSHIP

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SEAL:

SHEET TITLE:
AERIAL VIEWS

PROJECT NO:	2610
DRAWN BY:	
CHECKED BY:	
DATE:	11/15/16
SCALE:	1:1
SHEET:	

DP-5.1.2



1 AERIAL VIEW FROM SOUTH-WEST
SCALE: 1:1



2 AERIAL VIEW FROM NORTH-WEST
SCALE: 1:1



3 AERIAL VIEW FROM NORTH-EAST
SCALE: 1:1



4 AERIAL VIEW FROM SOUTH-EAST
SCALE: 1:1

Attachment 3

DP AREA GUIDELINES

#2 Queensborough Light Industrial and Mixed Employment

The Queensborough Light Industrial and Mixed Employment industrial areas, identified as Development Permit Area #2 [see Map C], are designated in order to provide areas of light and “ultralight” industrial and commercial land use as a transition between heavier industrial areas and residential areas.

Mixed Employment areas will include light industrial, office and other related employment uses. The only residential development permitted is that which is ancillary to businesses (i.e. caretaker suites) on these properties. Retail is not permitted unless it is ancillary to another permitted use. Light Industrial areas will include light industrial uses. This Development Permit Area encourages best practices for promoting water and energy conservation and reducing greenhouse gas emissions. It also establishes guidelines for the form and character of industrial and commercial development.

DESIGN GUIDELINES

ARRIVAL POINTS

Howes Street between Highway 91A and Westminster Highway/Boyd Street is the main arrival point to the business area of Queensborough. Properties within this development permit area that have at least one property line along Howes Street must comply with the guidelines in this section, in addition to complying with the other guideline sections of this Development Permit Area.

Building siting and massing must help to create a sense of arrival and a more pedestrian scale environment on Howes Street. Buildings must give the impression of “fronting” onto Howes Street although vehicle and pedestrian access may actually be taken from an alternative street. Consider the following:

- Locate buildings toward the Howes Street property line with off-street surface parking behind the buildings (i.e. on the side of the building facing away from Howes Street).
- Locate and design buildings to address both frontages at the corner of the intersection of Howes Street and Westminster Highway/Boyd Street.
- Use a building-height to right-of-way width proportion that reinforces a pedestrian scale streetscape.
- Create a cohesive streetscape by using a similar alignment of windowsills, building and roof lines, cornices, floor-to-floor spacing along the street block.

Design facades and windows to contribute to an interesting, pedestrian friendly environment. Street level windows that provide visual penetration into the building must be integrated into facades fronting onto Howes Street.

Consider the following:

- Use architectural elements (e.g. fenestration, vertical and/or horizontal design elements, secondary roof elements) and/or material or colour change to enliven the façade.
- Ensure blank walls do not occupy over 50% of the frontage onto Howes Street, and a section of blank wall does not exceed six linear metres (20 linear feet) without being interrupted by a window or entry (if applicable).
- Design windows to encompass a minimum of 40% and a maximum of 80% of the building front linear frontage.
- Use windows which are of clear glass (e.g. not tinted, reflective or opaque).
- Use windows which are rectangular or square in proportion, except for accent windows which may have a unique shape.
- Locate showrooms or other active uses (e.g. manufacturing activities) where they will provide visual interest for passing pedestrians.

Buildings must have a “signature” character. Consider integrating feature architectural elements or other signature elements.

Each development must provide within their property along Howes Street a publicly accessible sidewalk or multiuse pathway separated from the street by a landscaped boulevard with street trees.

CHARACTER

All buildings and developments must be designed to have a high quality, cohesive appearance that enhances the overall character of Queensborough.

Consider the following:

- Use an architectural approach (i.e. massing, facade treatment, detailing, materials and colour choice) which is harmonious with the riverfront community context.
- Design all principal and accessory buildings within a development and/or all elements of an individual building, to the same architectural style.

Provide public art to help enrich outdoor spaces and create pedestrian scale landmarks. Use art that highlights Queensborough’s sense of place and is unique to each location.

SITING

The layout of industrial operations, including truck access and egress and open loading areas must be designed to mitigate industrial emissions and noise impacts on adjacent residential land uses. Consider the following:

- Site buildings to facilitate the location of loud industrial activities (e.g. loading and unloading) away from residential areas.

- Ensure outdoor storage containers and/or goods stored in the open are visually unobtrusive.
- Provide landscape buffers at the site edge adjacent to residential land uses. Protect landscape buffers from industrial operations with a fence.

Building siting must respect the existing neighbourhood and site context.

Consider the following:

- Site buildings to retain and enhance heritage assets by incorporating them into the development of the site, wherever possible, including buildings, engineering works and/or cultural landscapes, as well as significant landscape features (e.g. mature vegetation and trees, distinctive landforms).
- Design new buildings in proximity to heritage assets to be compatible with their historical context without literally imitating older building styles. In these cases, new buildings should provide an original interpretation of the traditional building style (i.e. draw inspiration from fundamental design characteristics) while continuing to reinforce traditional development patterns and rhythms.
- Minimize the impact of noise and exhaust to pedestrians and neighbours. Locate service areas and mechanical equipment (e.g. utilities, HVAC, meters) at the rear of buildings and away from neighbouring residential uses. Minimize visibility of service areas and mechanical equipment from streets, open spaces and neighbours (e.g. screen, reduce service and garage opening size, use shared service areas).

HERITAGE

Each development must follow the Standards and Guidelines for the Conservation of Historic Places in Canada for all physical work to heritage assets.

Reuse historic industrial and agricultural artefacts on redevelopment sites (e.g. as public art).

ROOFS

Rooftops must appear clean and attractive and in keeping with the architectural style of the building. Consider the following:

- Locate and screen mechanical and service equipment such that it appears as an integral part of the building when viewed from any angle.
- Finish the surface of roofs with a material that is attractive and easy to maintain to a high level of neatness.
- Design roofs to reduce the urban heat island effect.

FACADES

The facades of all building walls that face public or internal streets, drive aisles, pedestrian pathways, parks or open space must provide visual interest. Use architectural elements (e.g. fenestration, vertical and/or horizontal design elements, secondary roof elements) and/or material or colour change.

ENTRANCES

Primary pedestrian entrances into buildings must be integrated into the design of the building, yet be clearly expressed. Consider the following:

- Articulate massing to identify building entrances (e.g. tall voids, central mass, recessed entry).
- Frame with a secondary roof element (e.g. porch) to identify building entrances and protect from weather.

WINDOWS

Use strategies to facilitate passive heating in cooler months and reduce unwanted heat gain in summer months. Consider the following:

- Ensure a solar heat gain coefficient of 50% or better for south facing windows to maximize solar gain during winter.
- Use exterior shading devices (e.g. awnings, canopies, overhangs, light shelves, louvers) which provide shade from the high summer sun, but provide solar access to the low winter sun. Use these devices particularly on south facing windows.

MATERIALS & COLOURS

All principal and accessory buildings within a development must use a cohesive palette of materials and colours that is consistently applied and contributes to the overall quality of the community. Consider the following:

- Use an industrial palette of wood, concrete, metal or brick and muted paint colour tones (e.g. Benjamin Moore's Historical Vancouver True Colours).
- Consistently apply materials to all sides of a building (i.e. do not emphasize the principal facade with lesser treatment on the other facades).
- Change building materials and/or colours at interior or "reverse" corners of a building, not at exterior corners or at changes in a facade plane.
- Use details (e.g. reveals in concrete buildings) and accent colours to highlight architectural elements (e.g. building entry) and provide visual interest. Use an accent colour which is harmonious with the main colours of the materials and colours palette.
- Use matte finishes or finishes with a low level of reflectivity. Reflective materials (e.g. mirrored glass, polished stone) should be avoided.

Each development must use building and hardscape materials that are durable and appropriate to their use, the local climate, and the urban environment.

OPEN SPACE

Each development must provide outdoor space for use by employees. Design to be of a usable size and configuration. Consider the following:

- Hard and soft landscaped areas such as courtyards, patios, lawns and/or naturalized open space.
- Seating options such as benches, moveable chairs and/or tables. Provide seating options suited to different weather conditions such as areas that capture the sun, are shaded (e.g. by building canopies or trees) and/or are sheltered from wind and rain.

Each development must provide pedestrian circulation that connects between work areas and employee open space.

TREES & PLANTING

Each development must use the BC Society of Landscape Architects' and BC Landscape and Nursery Association's "BC Landscape Standard Guidelines (Latest Edition)" in specifying, selection, site preparation, installation and maintenance of all trees and other plant materials.

Each development must integrate trees, including shade trees Consider the following:

- Retain existing mature trees wherever possible. Where tree removal is unavoidable, replace with a number, species and size of trees that creates equal value.
- Plant new trees in all employee parking and open space areas.
- Locate deciduous trees on the south and west side of buildings to provide shade and minimize unwanted heat gain during summer and provide solar access and passive solar gain during winter.

Tree species and other plant materials must be of high quality, suited to their purpose and contribute to the overall quality of the community. Consider the following:

- Choose species that are successful in the urban environment, easy to maintain, are non-invasive and suited to Queensborough's high water table. Selected tree species should also have less aggressive rooting habits.
- Use broadleaf deciduous tree species, wherever possible, for all shade trees including trees in parking areas. Select species that have a minimum mature height of 15 meters (49 feet).

Plant all trees so that they will successfully become established and develop a full canopy over time. Consider the following:

- In parking areas, plant shade trees at an approximate ratio of one tree for every five spaces. Plant trees in a minimum 3 metres (9.8 feet) wide continuous trench and protect trees with bollards or tree guards.

Develop and/or enhance areas of understorey vegetation using diverse, multistorey planting which will support habitat for smaller wildlife, songbirds and important pollinators such as bees, butterflies and dragonflies

SAFETY

Each development must provide a Crime Prevention Through Environmental Design (CPTED) report outlining the use of CPTED strategies in the design of developments and buildings, including open space.

TRAILS & GREENWAYS